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|-------------------|------------|
| Number            | YSNW 08/24 |
| Date of Issue     | 29/10/2024 |
| Amendment Number  | 0          |
| Date of Amendment |            |

## DEPARTMENT OF DEFENCE

### AERODROME METHOD OF WORKING PLAN (MOWP)

|                            |   |
|----------------------------|---|
| <b>AERODROME</b>           | <b>HMAS ALBATROSS (YSNW)</b>  |
| <b>PROJECT DESCRIPTION</b> | <b>P0010 - National Airfield Works at HMAS Albatross - RWY03 Displacement</b> |
| <b>IMPORTANT DATES</b>     |   |
| Approval of MOWP           | 31/10/2024  |
| Commencement of Works      | 04/11/2024  |
| Completion of Works        | 04/12/2024  |
| Expiry of MOWP             | 25/12/2024  |
| <b>CONTENTS</b>            |   |

|  |    |
|--|----|
| 1. WORKS INFORMATION.....                    | 2  |
| 2. RESTRICTIONS TO AIRCRAFT OPERATIONS.....  | 7  |
| 3. RESTRICTIONS ON THE WORKS CONTRACTOR..... | 9  |
| 4. ADMINISTRATION.....                       | 14 |
| 5. AUTHORITY.....                            | 17 |

|                |                                       |
|----------------|---------------------------------------|
| <b>ANNEXES</b> |                                       |
| A.             | Works Site, Access Route and Drawings |
| B.             | NOTAM Text                            |
| C.             | Distribution List                     |
| D.             | Notes                                 |
| E.             | AGL Isolation                         |
| F.             | GHD Technical/Design MEMO             |

## 1. WORKS INFORMATION

### 1.1. Description of Works

The works to be completed under this Method of Working Plan (MOWP) comprises of the following key construction activities:

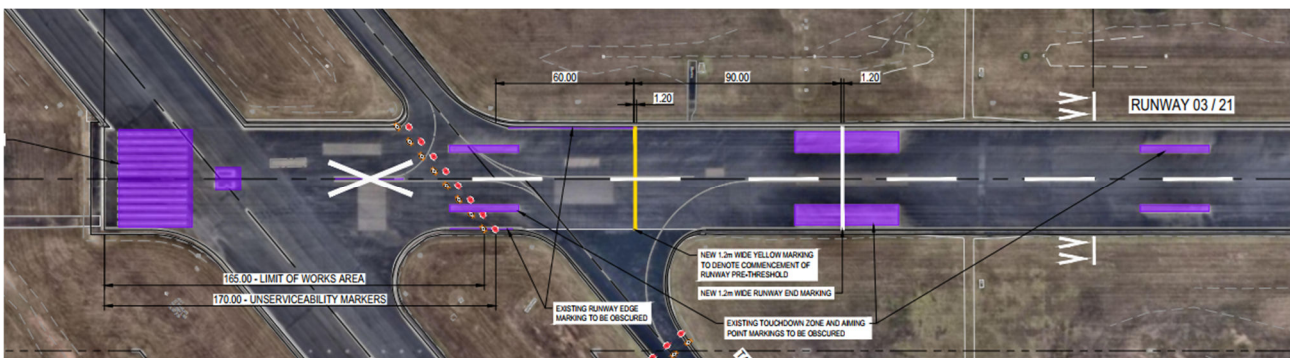
- Setup of RWY 03/21 Displacement for a 4 weeks duration;
- Isolation of existing AGL (See Annex E)
- Install pit and duct network for new AGL lighting incl. SIT/D25 and D7 pits;
- Install RWY edge lights and inset lights;
- Asphalt profiling, repair of existing rigid pavement and placement of new asphalt;
- Pavement Linemarking.

### 1.2. Implementation of Temporarily Displaced Threshold (< 30 days)

Setup of temporarily displaced threshold (<30 days) as per Manual of Standards Part 139 (MOS139) Cl. 8.29. Displacement distance of **429m**.

- o Placement/removal of aerodrome markers including temporary displaced threshold markers (<30 days), unserviceability cones, work limit cones and "X" markers;
- o Placement of temporary unserviceability solar lights where required;
- o Placement of temporary elevated blue edge lights on Taxiways where required;
- o Placement of temporary threshold lighting (solar);
- o Line marking install and removal as required;
- o Obscure RWY 21 DTRM's
- o Isolate PAPI03

**TORA/TODA/STODA/LDA/ASDA as per GHD's design (See Annex F)**



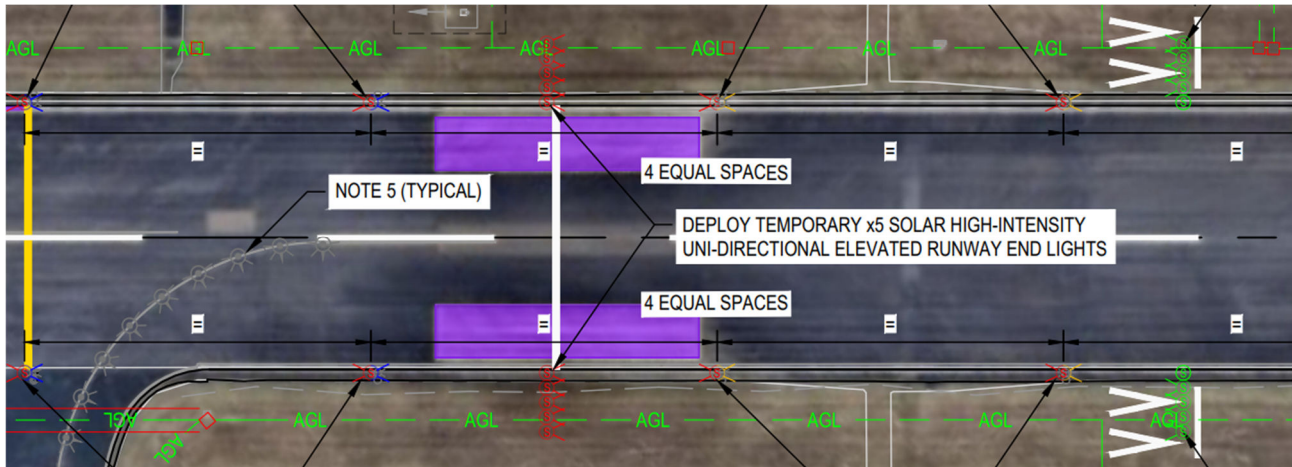


Figure 1 - Runway Intersection Work Area

**8.29 Temporarily displaced threshold markings — more than 5 days to 30 days or less**

- (1) If:
- (a) the permanent runway threshold is to be displaced for more than 5 days but not more than 30 days; and
  - (b) the runway is not serving scheduled international air transport operations; and
  - (c) RTIL are not provided in accordance with subsection 8.27 (3);
- then the temporarily displaced threshold markings must consist of the following:
- (d) “Vee-bar” markers comprising gable markers coloured white and positioned perpendicular to the centreline on each side of the runway, together with flush, white arrow markings in the landing direction, as shown in Figure 8.29 (1);
  - (e) for runways more than 18 m wide, or those serving air transport operations — 2 gable markers abutted end-to-end and positioned perpendicular to the centreline, and 2 arrow markings in the landing direction, must be provided on each side of the runway;

**Figure 8.29 (1) Temporarily displaced threshold markings for a runway not serving scheduled international air transport operations (30 days or less) (shows matters)**

### 1.3. Scope of Works

#### AGL Scope of Works

The AGL scope of works involves replacing the entire existing system with new LED lighting and generally involves working in all areas of the closure including the existing ALER.

The AGL installer will co-ordinate with EMOS to isolate relevant AGL circuits to safely enable construction activities within the closure.

- Isolate existing circuits where required (See Annex
- Demolition of existing and construction of new pit/duct system for AGL;
- Duct bank as required;
- Installation of Primary and Secondary circuits for AGL, incl. saw cuts for secondary cabling;
- Installation of temporary taxiway blue edge lights;

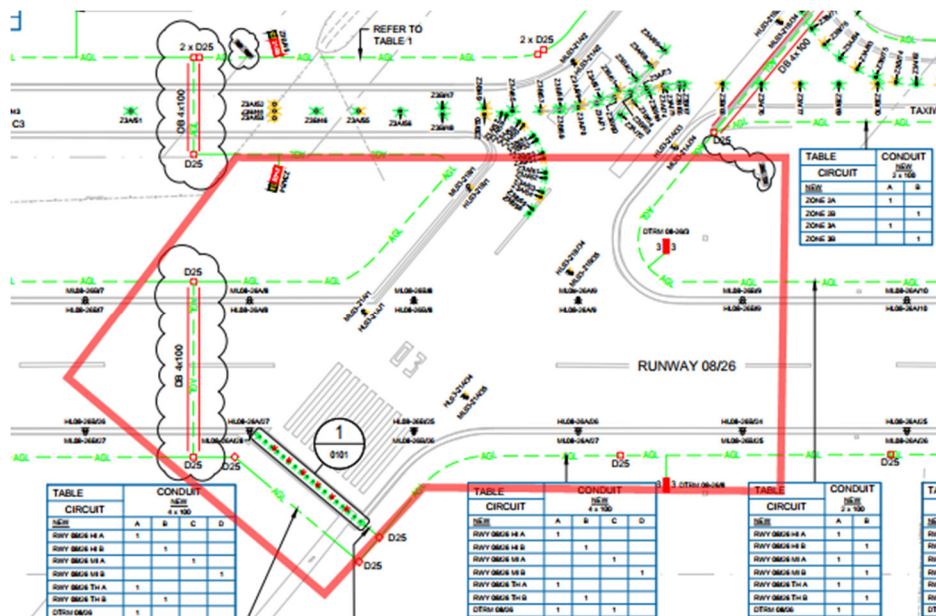


Figure 2 - RWY Intersection AGL Scope of Works (Red Box)



## Pavements Scope of Works

### Intersection of RWY 03/21 & 08/26 from THR03 to RWY 08/26 Graded Strip

- Profile and remove asphalt down to existing concrete base
- Prepare surface and place high strength grid;
- Placement of Asphalt (intermediate and wearing course);
- Install new pavement marking;
- Flanking and revegetation as required.

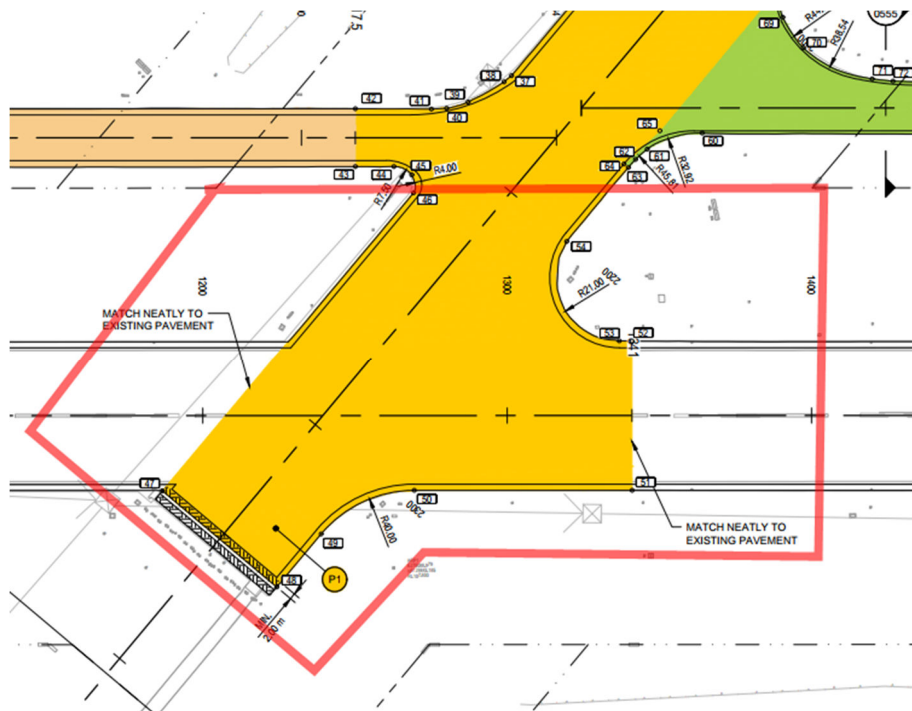


Figure 3- RWY Intersection Pavement Scope of Works (Red Box)

### 1.2.2 Works Program

All works will be conducted between Mon-Fri 0600-1800, unless prior approval is obtained from Defence to suit operational requirements.

### Works to be undertaken between RWY 08/26 Closure and RWY 03/21 Closure



### 1.2.3 Timing

#### Time of Completion

The specific work activities are expected to commence on **04/11/2024** and be substantially completed by **04/12/2024** for a total construction period of approximately **4** weeks with allowance for inclement weather. The actual date and time of commencement will be advised by a NOTAM, to be issued not less than 48 hours before work commences.

Upon completion of the works as described in this MOWP, BMD is to inform the PMCA in writing of the date of completion. If the works are completed ahead of the scheduled timeline, The Contractor is to inform the BAEO so that any extant NOTAMS can be withdrawn. Activities will occur mainly during day shifts (Monday to Saturday). Typically, 5 dayshifts per week.

Co-ordination and liaising with other Subcontractors carrying out work on the Base is to be communicated on a daily basis to minimise clashing of work activities and respective closures. The Contractor will assign a resource dedicated to directly manage coordination and subsequent security applications to ensure Defence have a single point of contact throughout the works.

## 2. RESTRICTIONS TO AIRCRAFT OPERATIONS

### 2.1. General

The works have been planned to minimise the impacts to Defence and civilian operations however works will require a **temporarily displaced threshold at RWY 03** for a duration of **4 weeks** to allow completion of construction activities. The Contractor will install adequately secured, temporary aerodrome markers and lighting IAW MOS 139 requirements for movement surface closures to ensure works and aerodrome user safety as per section 3.5.

### 2.2. Declared Emergencies and Adverse Weather

Under the direction of Air Traffic Control (ATC), Fire Controller or the HMAS Albatross Airfield Coordination Centre (ACC), the Works Safety Officer (WSO) is responsible for instructing the works contractor to vacate the work in the event of a declared emergency.

If time permits, ATC will advise the WSO the minimum time allowable to restore the airfield movement areas to operational condition. The WSO, in consultation with the Works Contractor, will then advise ATC whether restoration of the airfield movement areas is *possible or not possible*.

Outside ATC hours, the WSO will contact the Airfield Coordination Centre (through contact at Section 4.7) and advise the status of the works area and the ability for it to be made available for declared aircraft emergencies.

In the event of an emergency all personnel and equipment will vacate the airfield movement areas under the direction of ATC or the Fire Controller on SMCV or CTAF. The Primary Assembly Area is show below or at a location known to all personnel. The estimated time to vacate personnel and equipment from the work site is 15 min.



### 2.3. NOTAMs

NOTAM's will be required for this work.

LCDR Flying through ACC Ops will be responsible for issuing NOTAMs for each stage of the works. The Contractor is to liaise with the ACC Ops in regard to NOTAM requirements in the first instance.

The full planned NOTAM text for each stage of work is attached at Annex B. Dates and times are to be confirmed 48 hours before submission of any NOTAM and the commencement of works.

Any proposed changes to the works detail is to be carried out in consultation with the relevant stakeholders. Modification to the works information must be detailed in an amendment to this MOWP and the planned NOTAM text. All modifications must be reviewed and approved by the relevant authorities prior to the commencement of the works.

The final NOTAM text is to be released at least 48 hours prior to the commencement of the works.

Works are not to commence until the WSO has, on the day of the works, confirmed that the correct NOTAM is current for the planned works. The WSO is also to be in possession of hard copy of the published NOTAMs and MOWP relevant to their area of works.



### 3. RESTRICTIONS ON THE WORKS CONTRACTOR

#### 3.1. General

The Contractor shall comply with the requirements of the Contract Documents produced for this project, Manual of Standards 139, HMAS Albatross Aerodrome Manual and this MOWP. In the event of ambiguity or conflict of requirements, this MOWP takes precedence.

The Contractor is to prevent material from being blown onto aircraft movement areas by watering or other appropriate control measures.

On completion of each work period and on each occasion the movement area is returned to active use, the Contractor is to ensure the works area is made serviceable, including the removal of all FOD hazards, to the satisfaction of the WSO. The WSO is to advise the BAEO when the work area has been declared suitable for aircraft operations to enable an independent inspection to be conducted.

Smoking is not permitted on the airside movement area.

#### 3.2. Personnel

Only personnel with the correct valid security and Contractors pass will be permitted on site. All personnel associated with the work shall be bound by any instructions issued by the WSO, who may refuse access to persons likely, in his/her opinion, to compromise aircraft safety on the airfield.

Any breaches of aviation safety must be reported to the WSO. The WSO is to initiate an immediate response in consultation with ATC and the BASO. The BASO will provide guidance when an Aviation Safety Report (ASR) needs to be raised.

All personal and equipment will, when directed by the WSO, vacate the Runway and all taxiways and Aprons where works may conflict with aircraft movements.

All persons appointed to the project will undergo a site-specific induction on works safety and security requirements prior to commencing any works on site.

Additionally, the following provisions will apply to the Contractor:

- All personnel must remain within the boundaries of the work site at all times;
- All Contractors and personnel must comply with any additional security provisions, which may be imposed by the Department of Defence;
- Unauthorised sightseers or persons without a Defence Common Access Card (DCAC) or Visitor Identification Cards (VIC) must not enter the Security Restricted Area to visit the site;

- Unauthorised sightseers or persons without a Defence Common Access Card (DCAC) or Visitor Identification Cards (VIC) must not enter the Security Restricted Area to visit the site;
- Persons and vehicles may be subject to search at any time;

Personnel working airside must have completed Airside Awareness Training as a minimum and must remain under supervision/escort of Airside Access qualified personnel. Airside Awareness training is part of the Base Induction provided by Security and Estate Group, when first joining at the pass office. This however, does not supersede the requirement where site personnel with a Base Visitor's pass will still need to be escorted around the base by the WSO.

### Risks

- Aircraft jet suction
- Noise
- Jet blast and temperature.
- Plant/people interaction with Aircraft
- Obstacle Limitation Surface (OLS) penetration. Max allowable height is 4.88m (height of the MTV)
- FOD
- Aircraft landing and take off above work zone

### Controls

- Crew to evacuate to the original threshold when C17 is taxi-ing past or taking off the 03 direction
- Workers to have hearing protection available
- WSO to provide prior warning when large aircraft are incoming for landing/take-off.
- Area to be FOD free during works, a sweeper will be required to maintain area during works. Fixtures/cones to be pinned down and secure.

### 3.3. Vehicles and Plant

The highest vehicle or item of plant and equipment being utilised for these works is **4.88 metres in height**. Any higher piece of plant or equipment will not be established on site within the Works Area so as not to infringe the OLS.

No movement of vehicles or plant is to take place outside the works areas or access routes without being escorted by a WSO. Only vehicles and plant engaged in the work activities shall be permitted at the work site.

Vehicles and self-propelled items of plant are to have vehicle warning lights as detailed in CASA MoS Part 139 Section 9.19: Other Lights on an Aerodrome whenever they are operating within the airside movement area.

All vehicles and plant are to be clean prior to entering the airside active areas. All vehicles will undertake a FOD inspection prior to any entry airside.

### 3.4. Access to the Works and Security

Access to and from the works site will be as per Annex A and as directed by the WSO. The Contractor will have a separate detailed Vehicle Management Plan (VMP) for all airside access points, which will be provided separate to the MOWP. Movement of vehicles, plant and equipment must be confined to these routes to minimise tracking of dirt and debris onto aircraft movement area pavements and to prevent damage to aerodrome lighting.

Personnel are only permitted to move about the airside work areas under the direct and constant supervision of the WSO. Personnel are only permitted to move about the designated work areas, and only using the designated access routes. Any person detected on the site outside these limits may be evicted from the site and denied return.

Security is paramount at all times. All personnel are to have on their person a valid security pass issued by the HMAS Albatross Security Pass Office. All personnel requiring unescorted airside access to the works site must have an appropriate Airside Access Level as issued by the BASO post training. If personnel does not have airside access they must not access airside without a WSO.

### **3.5. Aerodrome Markers, Markings and Lights**

The Contractor is to provide all temporary markings, markers and lighting. Markings, markers and lighting are not to be installed, altered or removed without the approval of the WSO. The Contractor shall place aerodrome markers, markings and lights including, but not limited to orange cones / witches' hats, unserviceability cones and temporary lights in vicinity of the work zone demarking the extent of works in accordance with MOS Part 139 standards. Markers will comprise red steady lights (Night) and red banded white cones (Day) to define the limits of the available movement area and are depicted.

During the works, parts of the Aircraft Movement Area that are unserviceable due to the works will be marked and lit in accordance with the requirements of ICAO Annex 14 and MOS 139. The layout of unserviceability markings and lights is shown on the above figures for each Stage of work.

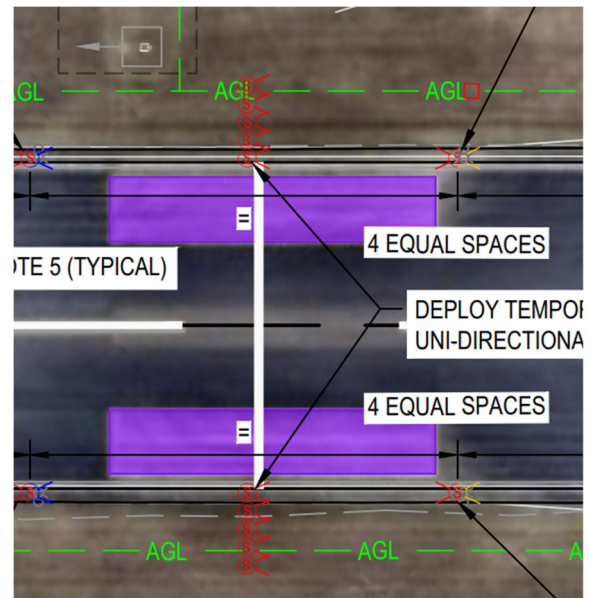
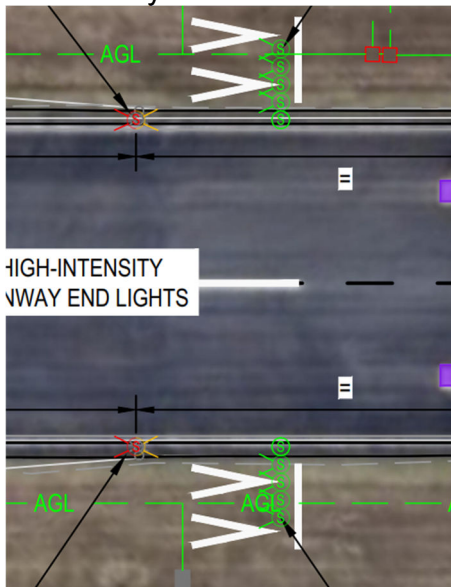
The layout for the installation, alteration and removal of aerodrome markers, markings and lights in the works area and other areas affected by the aerodrome works will be at the discretion of the WSO but will be in accordance with the requirements of MOS Part 139 (specifically Section 8) and the displacement design.

The Contractor shall place unserviceability cone markers and temporary lights at the designated locations and as directed by the WSO in order to mark the unserviceable areas of the movement area so that entry to the works areas by aircraft is prohibited. Unserviceability cones shall be placed by the Contractor to delineate the limits of the Aircraft Movement Area.

Red omni-directional lights shall be placed by the Contractor to delineate the limits of the Aircraft Movement Area. The lights shall be located at all locations where unserviceability cones are in place.

The displaced threshold (DTHR) will be marked with temporary line marking and lighting will be provided using 4no. green uni-directional solar powered lights placed on the both edges of the runway. 1no. Green omni-directional solar lights will be placed as the outer threshold lights on each side of the threshold.

A row of 5no. red uni-directional lights will be placed on each side of the runway to indicate the runway end.



All lighting within the displaced area or leading in/out of the displaced area will be extinguished. Blue temporary solar lights will be utilised for all taxiways where the existing lighting has been extinguished for removal.

The Contractor shall maintain all temporary lights in good working order and in the required locations and shall have sufficient spare new batteries in store on the airfield to ensure all temporary lighting is operational throughout its installation on the airfield.

The WSO is to liaise with the BAEO/BASO to ensure that markings are appropriate and that they are properly maintained at all times.

### 3.6. Protection of Electrical Services

The Contractor will undertake a thorough visual inspection of the work area to determine if any AGL lights and associated secondary cabling and all other electrical services are located in the vicinity of the work area. Services should be marked on service drawings and services can be traced back to SIT pits within the grassed areas. The Contractor will use locating wands to ascertain the presence of any underground electrical services. If service locations are detected, they are to be physically exposed using Non-Destructive Digging methods, recorded and then marked with paint and a physical barrier (such as traffic cones) are to be placed to mark their locations within the work area. If the service is unable to be relocated, the Contractor will ensure the service is protected during the works.



### 3.7. Instrument Landing System (ILS) Critical and Sensitive Areas

The Localiser located south of Runway 03 threshold will be unserviceable during the displacement works.



*Figure 4 - Critical Area - Glide Path*

### 3.8. Special Requirements

Any damage to the airfield or its equipment will be reported immediately to the WSO or BASO / LCDR Flying.

Prior to performing any hot work, the Works Contractor Works Supervisor is to obtain a Hot Work Permit.

Personal protective equipment (PPE) is to be worn at all times. Hearing protection will be worn in areas of close proximity to aircraft.

#### 3.8.1. Military Exercises and Expeditions

The Contractor has not been made aware of any upcoming military exercises that may impact the works.

### **3.8.2. Protection of Above Ground Aids**

The Contractor must protect all runway, taxiway and apron edge lights in each work area through the installation of approved barriers. The delineators must be placed prior to the commencement of work and must remain in place for the duration of the work period.

### **3.9. Dust and Foreign Object Damage (FOD) Control**

The Contractor is to ensure that aircraft pavements used or crossed during the works are kept clean and free of debris. Any debris is to be immediately removed by the Contractor.

All loose material and equipment are to be secured against movement in strong winds or aircraft blast. Measures shall be always taken for control of dust or other nuisance materials and the Contractor shall immediately respond to any direction by the WSO or the BASO to eliminate any problem. The Works Contractor is responsible for implementing appropriate control measures.

All above ground fixtures including but not limited to unserviceability cones will be pinned down to the surface to prevent risk of suction. Work limit cones and other temporary cones will be removed when a C17 or other large aircrafts are required to taxiway past the work site. Cones to be removed at end of shift.

The Contractor is to take every precaution to prevent any spillage of material on or in the vicinity of aircraft movement areas or in transit to and from the work site. Any spillage shall be removed by the Contractor.

On completion of each work period where airfield movement areas are required to be returned to active use, the Works Supervisor is to ensure the works area is made serviceable, including the removal of all FOD hazards.

## **4. ADMINISTRATION**

### **4.1. Project Manager**

The Project Manager / Contract Administrators (PMCA) are BECA, represented by Richie Hicks.

The Works Contractor Manager is BMD, represented by Project Manager, Matthew Hebden.

The Project Manager's contact details are at section 4.7.

### **4.2. Works Contractor**

The works will be carried out by BMD Constructions and their respective Subcontractors. All contact with the Base by the Works Contractor, in relation to the work, is to be made through the Project Manager.

### **4.3. Works Contractor Manager**

The Works Contractor Manager will be BMD (represented by Matthew Hebden). The Works Contractor Manager or representative must be present on site during all work periods.

The Works Contractor Manager is responsible, on behalf of the Works Contractor, for complying with the requirements of this MOWP.

The Works Contractor Works Manager's contact details are at section 4.7.

#### **4.4. Works Safety Officer**

Works Safety Officers (WSOs) will be supplied by Oz Airports and they will be present on site with the construction crew at all time during the works related to this project. The duties of the WSO are to escort the works crew and to monitor and control their movements to ensure the airside safety is maintained.

The WSO is responsible for ensuring that the works are conducted in accordance with CASA Manual of Standards (MOS) - Part 139 Aerodromes, CASA directions related to aerodrome works, directions from the aerodrome operator and this MOWP.

The WSO is responsible for the following:

- Ensure the safety of aircraft operations in accordance with the standards for aerodrome works and this MOWP;
- Ensure that, where applicable, the aerodrome works are notified by issue of a NOTAM and that the text of each NOTAM is as set out in the MOWP prior to works commencing on site;
- Initiating and cancelling work related NOTAMS in consultation with the BAEO;
- Supply ATC, on a daily basis, with information necessary to ensure the safety of aircraft operations;
- Maintain positive radio communications with ATC;
- Discuss with the Contractor, on a daily basis, any matters necessary to ensure the safety of aircraft operations;
- Brief the Contractor, staff and sub-contractors on the safety precautions required during the works. The brief is to include but not limited to conditions likely to affect the conduct of the works such as:
  - Weather.
  - Other NOTAM airfield activities.
  - Work site evacuation procedures including the signal to be employed.
- Ensure that unserviceable portions of the movement area, temporary obstructions and the limits of the works are correctly marked and lit in accordance with MOS 139, ADFP 602 and this MOWP.
- Ensure that the vehicles, plant and equipment carrying out aerodrome works are properly marked and lit or are under works safety officer supervision or within the properly marked and lit works area.
- Ensure that all other requirements of the directions and MOWP relating to vehicles, plant, equipment and materials are complied with.

- Ensure that access routes to work areas are in accordance with the MOWP and clearly identified and that access is restricted to these routes.
- Ensure that excavation is carried out in accordance with the MOWP and, in particular, so as to avoid damage or loss of calibration to any underground power or control cable associated with a precision approach and landing system or any other navigational aid.
- Report immediately to ATC and the BASO any incident, or damage to facilities, likely to affect air traffic control services or the safety of aircraft.
- Remain on duty at the works area while work is in progress.
- Ensure that ATC is kept informed of the radio call signs of the vehicles used by the works safety officer.
- Immediately remove vehicles, plant and personnel from the movement area where necessary to ensure the safety of aircraft operations and/or works personnel.
- Ensure that the movement area is safe for normal aircraft operations on completion of the work following removal of vehicles, plant, equipment, and personnel from the works area.
- In the case of any time-limited works (TLW) which may arise, ensure that the TLW area is restored to normal safety standards not less than 5 minutes before the time scheduled or notified for an aircraft movement unless varied by ATC.
- Ensure that floodlighting or any other lighting required for carrying out aerodrome works is shielded so as not to represent a hazard to aircraft operations.
- Carrying out WSO duties in accordance with the requirements as stipulated in MOS Part 139 Section 10.12.

#### **4.5. Base Aviation Safety Officer**

The BASO / LCDR Flying will provide Airside Access training and conduct inductions, if required. Base induction includes airside awareness which is to be completed as a minimum and WSOs are to be approved to level 3 Airfield Access at HMAS Albatross.



#### 4.6. Contact Details

| Appointment                              | Agency           | Contact Details  |
|--|------------------|--|
| Aerodrome Manager                        | HMAS Albatross   | LCDR Shane Firkin<br><a href="mailto:albatross.aioperations@defence.gov.au">albatross.aioperations@defence.gov.au</a>                    |
| Base Airfield Engineering Officer (BAEO) | HMAS Albatross   | LEUT Leigh Paulin<br><a href="mailto:albatross.aioperations@defence.gov.au">albatross.aioperations@defence.gov.au</a>                    |
| Base Airfield Safety Officer (BASO)      | HMAS Albatross   | LEUT Jack Fiddis<br><a href="mailto:albatross.aioperations@defence.gov.au">albatross.aioperations@defence.gov.au</a>                     |
| Airfield Coordination Officer            | HMAS Albatross   | LEUT Craig Daniel<br>(02) 4449 2161<br><a href="mailto:albatross.aioperations@defence.gov.au">albatross.aioperations@defence.gov.au</a>  |
| Airfield Coordination Officer            | HMAS Albatross   | Ms Rebecca Heather<br>(02) 4449 2161<br><a href="mailto:albatross.aioperations@defence.gov.au">albatross.aioperations@defence.gov.au</a> |
| Project Manager                          | BECA             | Richie Hicks<br>0444 531 535<br><a href="mailto:Richie.hicks@beca.com">Richie.hicks@beca.com</a>   |
| Contractor Works Manager                 | BMD Construction | Matthew Hebden<br>0428 078 752<br><a href="mailto:Matthew.hebden@bmd.com.au">Matthew.hebden@bmd.com.au</a>                               |
| Work Safety Officer                      | Oz Airports      | Robert Squires<br>0405 812 288<br><a href="mailto:Robert.squires@ozairports.com.au">Robert.squires@ozairports.com.au</a>                 |

## 5. AUTHORITY

### 5.1. Issue

This MOWP is issued in accordance with CASA Manual of Standards Part 139. All works will be carried out in accordance with the MOWP.

### 5.2. Variation


Minor variations to this MOWP can be issued by the Project Manager after liaising with the BASO. However, no variation to this MOWP of a substantial nature is to be issued without the approval of the signatories. These variations will be authorised and distributed as an amendment.

Requests or proposals to amend or vary the MOWP, as well as queries on its content or meaning, are to be directed to LCDR Flying through the ACC OPS.



**Approval**

Under the authority of the following signatures, this MOWP is approved for release.

| Operating Agent  | Base Manager  | Works Contractor  |
|--|---|---|
| <b>Shane FIRKIN</b><br>Digitally signed by Shane FIRKIN<br>Date: 2024.10.31 16:17:06 +11'00' | <b>Charma ine RICHARDS</b><br>Digitally signed by Charmaine RICHARDS<br>Date: 2024.11.01 08:55:06 +11'00' |  |
| Shane Firkin   | Charmaine Richards  | Matthew Hebden  |
| Aerodrome Manager  | Base Manager  | Project Manager   |
| Airfield Coordination Centre   | S&EG  | BMD Construction  |
| HMAS Albatross   | HMAS Albatross  |   |
| Date:  | Date:   | Date: 01/11/2024  |

**Annexes:**

- A. Works Site, Access Route and Drawings
- B. NOTAM Text
- C. Distribution List
- D. Notes
- E. Isolation Sheet

ANNEX A - Works Site, Access Route and Drawings



Figure 5 - Asphalt Batch Plant Location





Figure 6 - Access Route for Intersection Asphalt Works



**ANNEX B – NOTAMS Text (EXAMPLE)**

|                              |   |                  |
|------------------------------|---|------------------|
| <b>NOTAM ONE</b>             | <b>Zulu</b>   |                  |
| <b>A) Location</b>           | <b>YNSW AD</b>  |                  |
| <b>B) Start</b>              | <b>27/09/2024</b>                                       | <b>1100</b>      |
| <b>C) Finish</b>             | <b>25/10/2024</b>                                       | <b>2300</b>      |
| <b>D) Period of Activity</b> | <b>MON-SUN</b>  | <b>1100/2300</b> |
|                              |   |                  |
|                              |   |                  |
| <b>E) NOTAM Text</b>         | RWY 03 DISPLACED 439M DUE WIP<br><br>REFER MOWP 08/2024 |                  |

**ANNEX C - DISTRIBUTION LIST**

CMDR AIR HMAS ALBATROSS  
453 SQN DET NOWRA OPSCDR  
BASE MANAGER ALBATROSS  
PROJECT DIRECTOR  
PROJECT MANAGER  
WORKS CONTRACTOR  
WORKS SAFETY OFFICER

**ANNEX D - BAEO's Notes**

1. Maximising the TORA/TODA/ASDA/LDA for RWY 21

1. In order to accommodate Nowra's usual fixed-wing A/C, the RAAF C-17 and Air Affairs Learjet (LR35), the AD OPR seeks to achieve the longest TODA, etc. (i.e. RWY21-work zone-RESA) with a calculated gradient of greater than the current most restrictive obstacle being appropriate (i.e. > 6.85%).
2. Through consultation with aerodrome users, it is evident that the initial DTHR, as proposed by GHD, will impact the operational capability of the aerodrome. Both C-17 and LR35 operators will need to alter and/or relocate their operations, with the initial DTHR. Operators have indicated that the extending the available length, will aid in their ability to sustain ALB operations.
3. By providing a RWY end at 150m from the works zone, we will be meeting the minimum requirements of MOS139 through the provision of a 60m CWY and a 90m RESA. Operators will account this works/RWY separation through the application of the normal process, having being developed under the same ICAO principles.
4. AD OPR and BMD will implement a process, by which project WSOs will vacate the works zone of personnel for C-17 operations. If necessary, upon implementation of the DTHR, WSOs may vacate works zones for further A/C arrivals.

2. Maximising the TORA/TODA/ASDA for RWY 03

1. In order to accommodate Nowra's usual fixed-wing A/C, the RAAF C-17 and Air Affairs Learjet (LR35), the AD OPR seeks to achieve the longest TODA, etc.. To achieve this, the AD OPR is requiring the implementation of the starter extension, at a distance of 60m from the U/S cones of the works zone.
2. Through consultation with aerodrome users, it is evident that the initial DTHR, as proposed by GHD, will impact the operational capability of the aerodrome. Both C-17 and LR35 operators will need to alter and/or relocate their operations, with the initial DTHR. Operators have indicated that the extending the available length, will aid in their ability to sustain ALB operations.
3. At 60m, the C-17 will impact on the works zone, both personnel and light equipment. C-17 operators (36SQN Standards Team) are comfortable to operate the 60m, with the process outlined below. LR35 operations are not considered to have a significant impact on works zone, as such no specific changes to works operations are planned (pending WSO input after DTHR implementation).
4. AD OPR and BMD will implement a process, by which project WSOs will vacate the works zone of personnel for C-17 operations. If necessary, upon implementation of the DTHR, WSOs may vacate works zones for further A/C departures.
5. A/C operators will employ 'minimum' power to facilitate taxiing only, prior to crossing the starter extension line.
6. AD OPR has advised that 1x omni-directional blue light is required on each side of the RWY. The DTHR NOTAM will reflect this.

7. The AD OPR accepts the previously provided location for the DTHR, given it employs a 3.33% approach gradient and the 4.8m works height (adjusted for RWY slope). (I.e. LDA 03 will not be extended)
3. Minimising the gap between U/S markers and work limits markers within the DTHR works zone.
  1. AD OPR acknowledges that gap between is not to standard.
  2. The intention, as discussed with BMD and the WSO team, is for WSO to manage the works zone to maximise the available work area, while maintaining access to TWY C. ALB will remain a operational airfield for the duration of the works and a open and constant communication pathway between AD OPR and project personnel will be key to the safe operation in this area.
4. Isolation of the RWY 03 PAPIs and RWY DTRM
  1. AD OPR acknowledges the intention to isolate the RWY 03 PAPIs and the RWY 03/21 DTRMs. A separate NOTAM will reflect this.
5. Non-availability of RWY 21 ILS
  1. During the implementation of the DTHR, the RWY 21 ILS will be NOTAM'd as not available.

**ANNEX E - AGL Isolation Requirements**

| HMAS Albatross            |                 |                     |                        |                                  |                |  |                    |           |             |           |
|---------------------------|-----------------|---------------------|------------------------|----------------------------------|----------------|--|--------------------|-----------|-------------|-----------|
| AGL & CIRCUIT ISOLATION/S |                 |                     | MOWP: YSNW 08/24       |                                  |                |  | Required Date/Time |           |             |           |
| Work ID                   | Work Location   | Circuit Isolations  | CCR No. Based on AGLCM | AGL NO.                          | Isolation Type | Work description   | Start time         | Date      | Finish time | Date      |
| 1                         | RWY 08/26       | RWY 08/26 E         | 10                     | RWE/9                            | Partial        | CCR for Circuits RWY 08/26 E to be turned off temporarily<br>Sits to be shorted in the associated SIT Pit Location .<br>Light fittings to be removed for pavement work.      | 0700               | 1/11/2024 | 1700        | 1/11/2024 |
|                           |                 |                     |                        | RWE/10                           | Partial        |  |                    |           |             |           |
| 2                         | RWY 08/26       | RWY 08/26 H         | 28                     | RWH/7                            | Partial        | CCR for Circuits RWY 08/26 H to be turned off temporarily<br>Sits to be shorted in the associated SIT Pit Location .<br>Light fittings to be removed for pavement work.      | 0700               | 1/11/2024 | 1700        | 1/11/2024 |
|                           |                 |                     |                        | RWH/8                            | Partial        |  |                    |           |             |           |
| 3                         | RWY 03/21       | RWY 03/21 B         | 23                     | RWB/17+03/21<br>Threshold Lights | Partial        | CCR for Circuits RWY 03/21 B to be turned off temporarily<br>Sits to be shorted in the associated SIT Pit Location .<br>Light fittings to be removed for pavement work.      | 0700               | 1/11/2024 | 1700        | 1/11/2024 |
|                           |                 |                     |                        | RWB/18                           | Partial        |  |                    |           |             |           |
| 4                         | RWY 03/21       | RWY 03/21 A         | 6                      | RWA/17+03/21<br>Threshold Lights | Partial        | CCR for Circuits RWY 03/21 A to be turned off temporarily<br>Sits to be shorted in the associated SIT Pit Location .<br>Light fittings to be removed for pavement work.      | 0700               | 1/11/2024 | 1700        | 1/11/2024 |
|                           |                 |                     |                        | RWA/18                           | Partial        |  |                    |           |             |           |
| 5                         | RWY 03/21       | RWY 03/21 C         | 7                      | RWC/15+03/21<br>Threshold Lights | Partial        | CCR for Circuits RWY 03/21 C to be turned off temporarily<br>Sits to be shorted in the associated SIT Pit Location .<br>Light fittings to be removed for pavement work.      | 0700               | 1/11/2024 | 1700        | 1/11/2024 |
|                           |                 |                     |                        | RWC/16                           | Partial        |  |                    |           |             |           |
| 6                         | RWY 03/21       | RWY 03/21 D         | 24                     | RWD/15+03/21<br>Threshold Lights | Partial        | CCR for Circuits RWY 03/21 D to be turned off temporarily<br>Sits to be shorted in the associated SIT Pit Location .<br>Light fittings to be removed for pavement work.      | 0700               | 1/11/2024 | 1700        | 1/11/2024 |
|                           |                 |                     |                        | RWD/16                           | Partial        |  |                    |           |             |           |
| 7                         | RWY 03/21       | RWY 03/21<br>OT1    | 1                      | OT1/2                            | Partial        | CCR for Circuits RWY 03/21 OT1 to be turned off<br>temporarily<br>Sits to be shorted in the associated SIT Pit Location .<br>Light fittings to be removed for pavement work. | 0700               | 1/11/2024 | 1700        | 1/11/2024 |
|                           |                 |                     |                        | OT1/1                            | Partial        |  |                    |           |             |           |
| 8                         | PAPI03/PAPI 03A | PAPI 03,<br>PAPI 21 | 21                     | PAPI03/1                         | Full           | Lights to be isolated for the displacement. SITS to be shorted.<br>CCR for PAPI03,PAPI21 to be turned OFF. CCR to re-energise after the SITS for PAPI03 shorted.             | 0700               | 1/11/2024 | 1700        | 1/11/2024 |
|                           |                 |                     |                        | PAPI03/2                         |                |  |                    |           |             |           |
|                           |                 |                     |                        | PAPI03/3                         |                |  |                    |           |             |           |
|                           |                 |                     |                        | PAPI03/4                         |                |  |                    |           |             |           |



**ANNEX E TO  
MOWP YSNW 08/24**

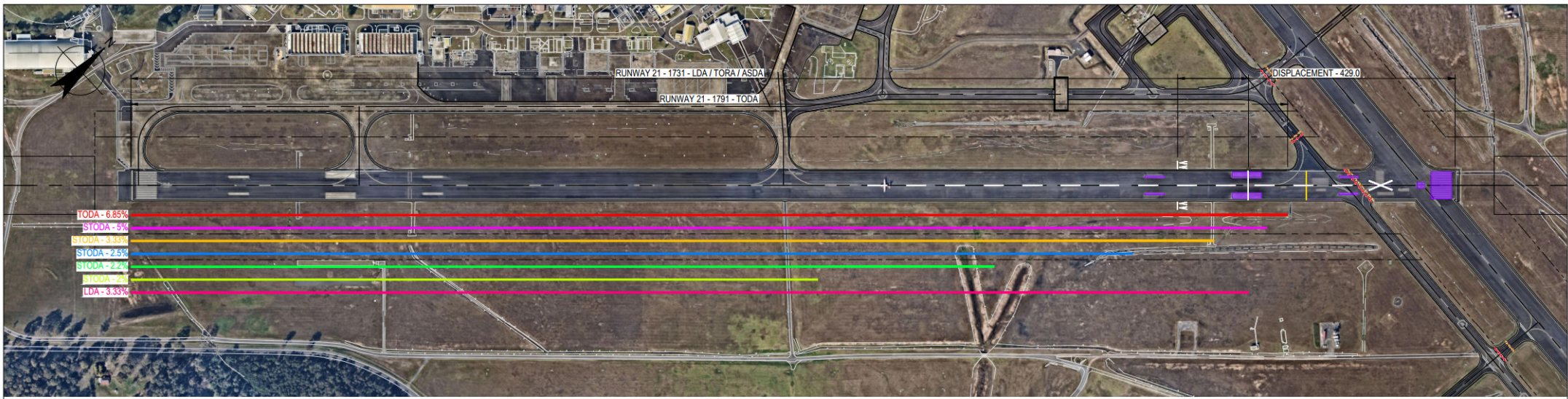
|  |             |                     |    |                |      |  |      |           |      |           |
|--|-------------|---------------------|----|----------------|------|--|------|-----------|------|-----------|
| 9  | TWY C1      | TWY C1,<br>Apron CA | 30 | Entire Circuit | Full | Existing Lights to be permanently isolated and replaced with temporary blue solar edge lights.CCR for TWY C1, Apron CA to be turned off. | 0700 | 1/11/2024 | 1700 | 1/11/2024 |
| 10   | Civil Apron | TWY C1,<br>Apron CA | 30 | Entire Circuit | Full | Existing Lights to be permanently isolated and replaced with temporary blue solar edge lights.CCR for TWY C1, Apron CA to be turned off. | 0700 | 1/11/2024 | 1700 | 1/11/2024 |
| 11   | DTRM 03/21  | DTRM<br>03/21       | 25 | Entire Circuit | Full | To be isolated for the displacement. Will not be re-energised at end of displacement   | 0700 | 1/11/2024 | 1700 | 1/11/2024 |
| Individual AGL Affected - Access to above Lighting pits will be required |             |                     |    |                |      |  |      |           |      |           |

**ANNEX F - GHD TECHNICAL AND DESIGN MEMO**

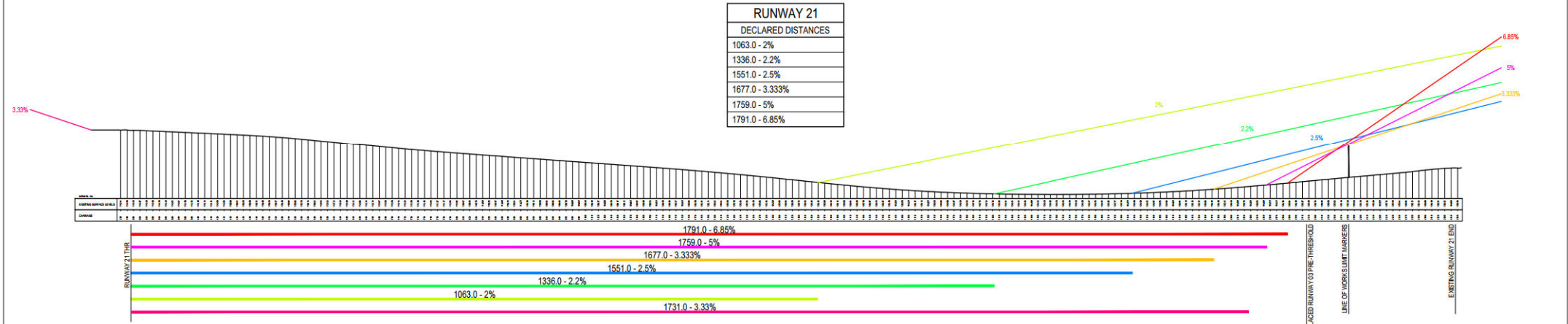
# 12541981-CAN-PDP

## Consultant Advice Notice

|  |   |                        |                   |                        |                        |
|--|---|------------------------|-------------------|------------------------|------------------------|
| <b>CAN NO:</b>                                 | 036   | <b>Project no.</b>     | 12541981          |                        |                        |
| <b>Project name</b>                            | P0010 – National Airfield Works   |                        |                   |                        |                        |
| <b>Client</b>                                  | Department of Defence   | <b>Date</b>            | 31 October 2024   |                        |                        |
| <b>PMCA</b>                                    | Beca  | <b>Discipline</b>      | Design Management |                        |                        |
| <b>Attention</b>                               | <i>richie.hicks@beca.com</i>  | <b>CAN Prepared by</b> | Phyrylyne Cabato  |                        |                        |
| <b>Distribution</b>                            | <i>neely.majlessi@beca.com; lindsay.murray@beca.com; pete.muir@beca.com</i>                                 |                        |                   |                        |                        |
| <b>Subject</b>                                 | 12541981-CAN-PDP-036 – ALB Displaced Threshold Memo and Sketches  |                        |                   |                        |                        |
| <b>Reason</b>                                  | In response to VPR 003a   |                        |                   |                        |                        |
| <b>Directed Changes and Updates GHD advice</b> | <b>GHD advice is as follows: -This notice is <u>not</u> an instruction under the construction contract.</b> |                        |                   |                        |                        |
|  | Please find below a summary of documents issued with this Consultant Advice Notice.                         |                        |                   |                        |                        |
|  | <b>Ref.</b>   | <b>Document</b>        | <b>Rev</b>        | <b>Site</b>            | <b>Advice</b>          |
|  | 1   | 12541981-01-G-SK-3001  | B                 | ALB                    | In response to VPR003a |
| 2  | 12541981-01-G-SK-3002   | B                      | ALB               | In response to VPR003a |                        |
| 3  | 12541981-01-G-SK-3003-B   | B                      | ALB               | In response to VPR003a |                        |
| 4  | 12541981-01-G-SK-3004-B   | B                      | ALB               | In response to VPR003a |                        |
| 5  | 12541981-01-G-SK-3005-B   | B                      | ALB               | In response to VPR003a |                        |
| 6  | 12541981-01-G-SK-3006-B   | B                      | ALB               | In response to VPR003a |                        |
| 7  | 12541981-01-G-SK-3007-B   | B                      | ALB               | In response to VPR003a |                        |
| 8  | 12541981-MEMO-ALB Displaced Threshold   | 2                      | ALB               | In response to VPR003a |                        |
|  |   |                        |                   |                        |                        |
| <b>Other comments</b>                          | Attachments: - Documents as listed above.   |                        |                   |                        |                        |
|  | Signed:<br><br><br><br><br><br><br><br><br><b>Ashkan Moghaddas</b><br>Project Manager                       |                        |                   |                        |                        |



**RUNWAY 21 - DECLARED DISTANCES AND SUPPLEMENTARY TAKE-OFF DISTANCES (STODA)**  
SCALE 1:3000



**RUNWAY 21 - SUPPLEMENTARY TAKE-OFF DISTANCES (STODA)**  
SCALE 1:3000

|                   |             |                |          |          |
|-------------------|-------------|----------------|----------|----------|
| B FOR INFORMATION |             | G.F.           | A.H.     | 31.10.24 |
| A FOR INFORMATION |             | G.F.           | D.E.     | 21.08.24 |
| Rev               | Description | Checked        | Approved | Date     |
| Author            | D. WALKER   | Drafting Check |          |          |
| Designer          | D. WALKER   | Design Check   |          |          |



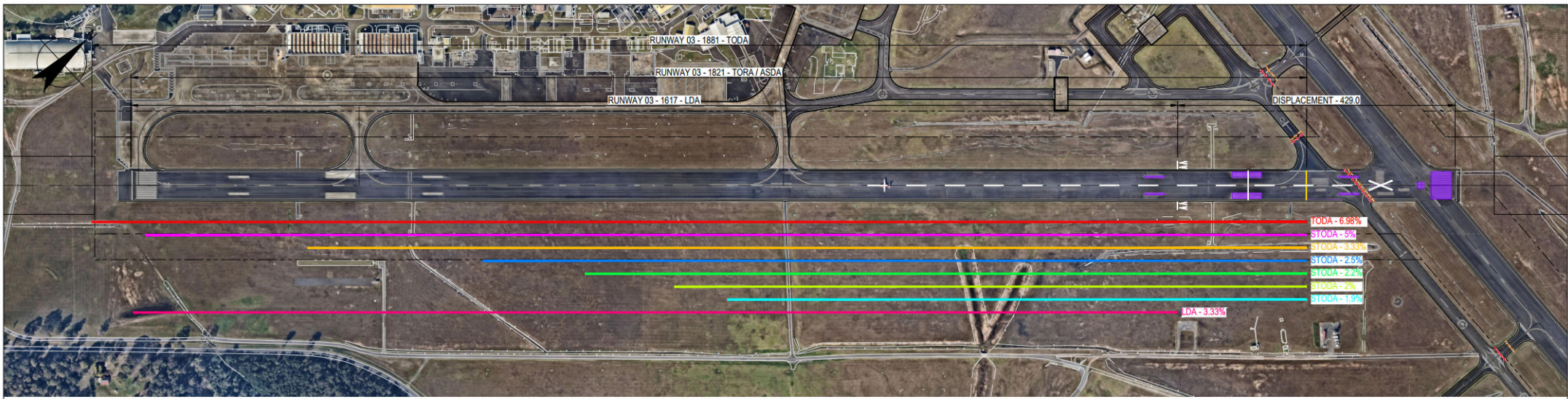
|             |                                     |
|-------------|-------------------------------------|
| Client      | DEPARTMENT OF DEFENCE SEG - CFI     |
| Project     | EST04824 - P0010 NAW HMAS ALBATROSS |
| Status      | PRELIMINARY                         |
| Project No. | 12541981                            |

Drawing Title: **DISPLACED THRESHOLD 03 DECLARED DISTANCES RUNWAY 21**

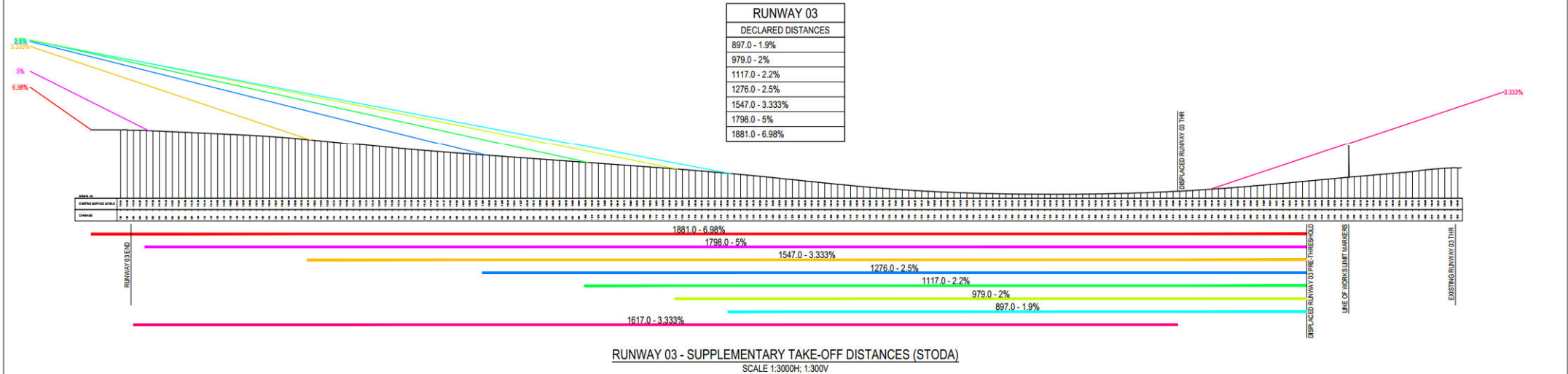
12541981-01-G-SK-3001

Size: **A1**  
Rev: **B**





**RUNWAY 03 - DECLARED DISTANCES AND SUPPLEMENTARY TAKE-OFF DISTANCES (STODA)**  
SCALE 1:3000



**RUNWAY 03 - SUPPLEMENTARY TAKE-OFF DISTANCES (STODA)**  
SCALE 1:3000H; 1:300V

|                   |             |                |          |          |
|-------------------|-------------|----------------|----------|----------|
| B FOR INFORMATION |             | G.F.           | A.H.     | 31.10.24 |
| A FOR INFORMATION |             | G.F.           | D.E.     | 21.08.24 |
| Rev               | Description | Checked        | Approved | Date     |
| Author            | D. WALKER   | Drafting Check |          |          |
| Designer          | D. WALKER   | Design Check   |          |          |

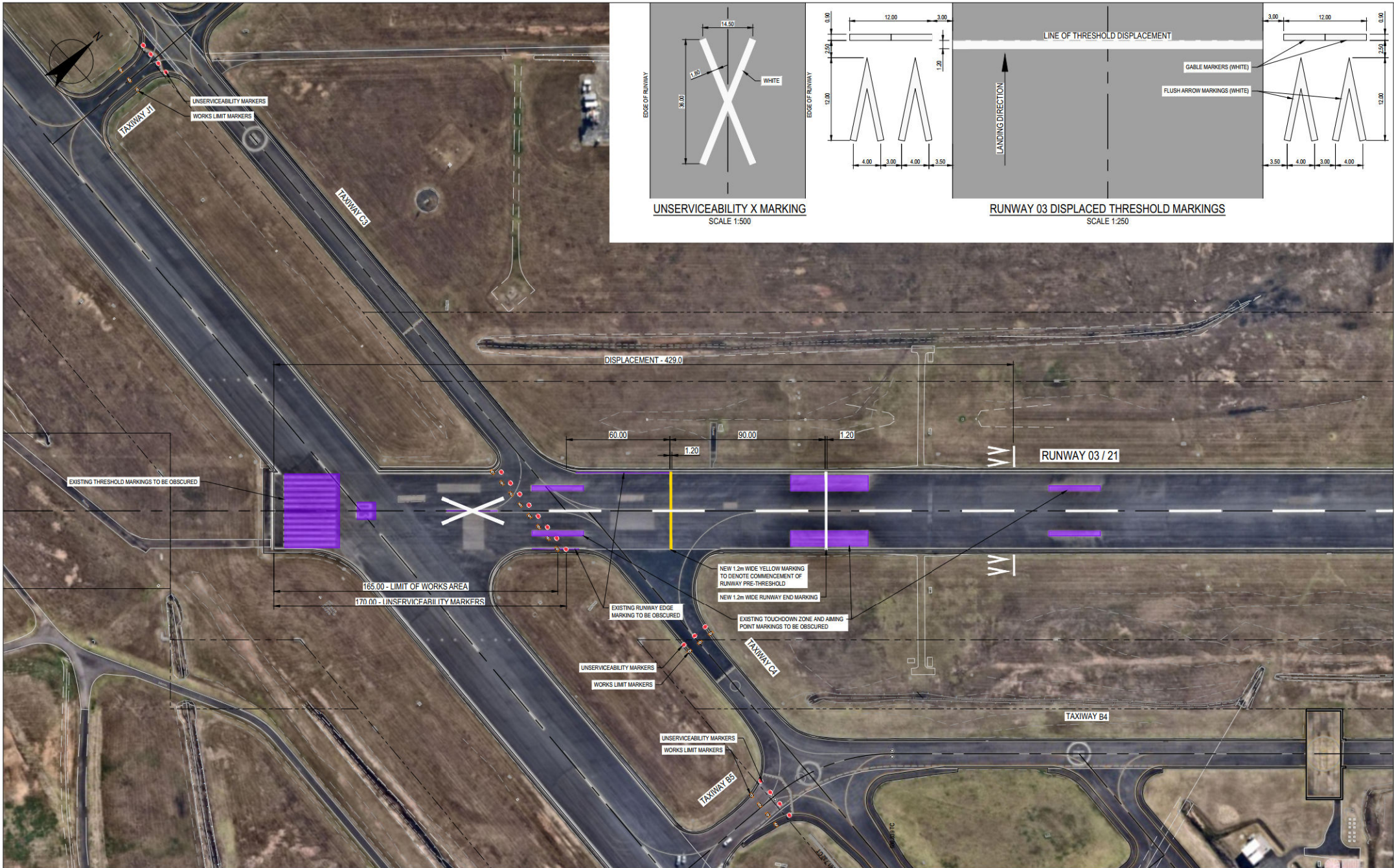


|             |                                     |
|-------------|-------------------------------------|
| Client      | DEPARTMENT OF DEFENCE SEG - CFI     |
| Project     | EST04824 - P0010 NAW HMAS ALBATROSS |
| Status      | PRELIMINARY                         |
| Project No. | 12541981                            |
| Status Code |                                     |

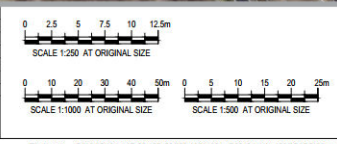
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|---------------|---|
| Drawing Title | DISPLACED THRESHOLD 03 DECLARED DISTANCES RUNWAY 03 |
| Drawing No.   | 12541981-01-G-SK-3002                               |
| Rev           | B   |

Size A1





|                   |             |                |          |          |
|-------------------|-------------|----------------|----------|----------|
| B FOR INFORMATION |             | G.F.           | A.H.     | 31.10.24 |
| A FOR INFORMATION |             | G.F.           | D.E.     | 21.08.24 |
| Rev               | Description | Checked        | Approved | Date     |
| Author            | D. WALKER   | Drafting Check |          |          |
| Designer          | D. WALKER   | Design Check   |          |          |



Client

**Australian Government**  
Department of Defence  
Security and Estate Group

PMCA

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E admln@ghd.com.au W www.ghd.com

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Client **DEPARTMENT OF DEFENCE SEG - CFI**

Project **EST04824 - P0010 NAW  
HMAS ALBATROSS**

Status **PRELIMINARY**

Project No. **12541981**

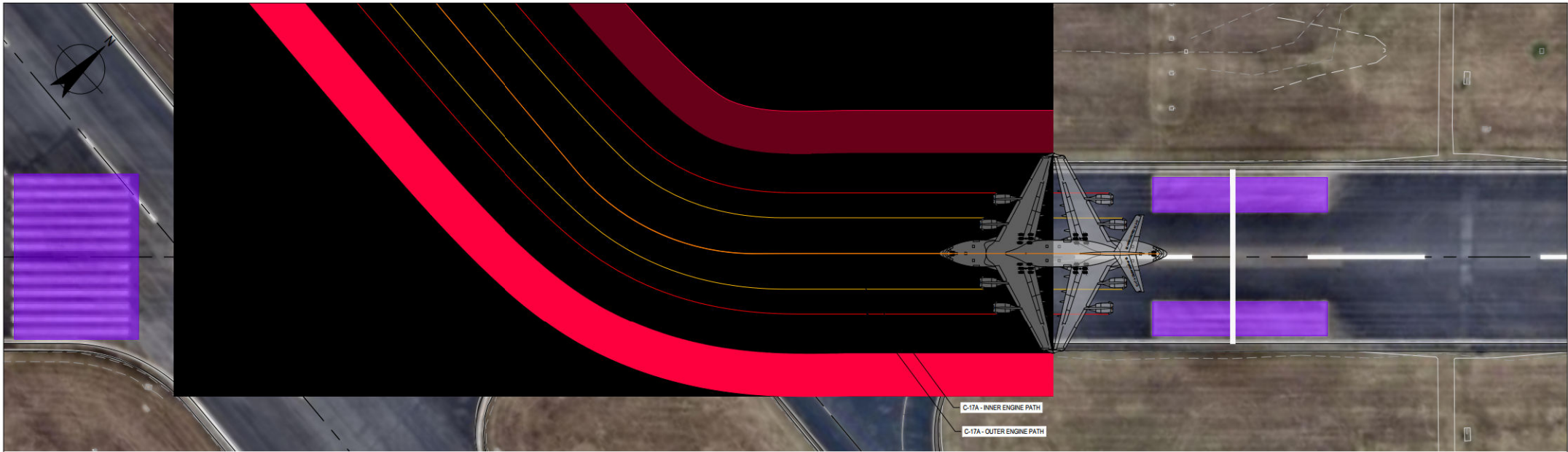
Drawing Title **DISPLACED THRESHOLD  
NAVIGATION AIDS & MARKINGS  
RUNWAY 03**

Drawing No. **12541981-01-G-SK-3003**

Size **A1**

Rev **B**





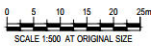
C-17A - INNER ENGINE PATH  
C-17A - OUTER ENGINE PATH

AIRCRAFT TRACKING - C-17A  
SCALE 1:500



AIRCRAFT TRACKING - LEARJET 35  
SCALE 1:500

|                   |             |                |          |          |
|-------------------|-------------|----------------|----------|----------|
| B FOR INFORMATION |             | G.F.           | A.H.     | 31.10.24 |
| A FOR INFORMATION |             | G.F.           | D.E.     | 21.08.24 |
| Rev               | Description | Checked        | Approved | Date     |
| Author            | D. WALKER   | Drafting Check |          |          |
| Designer          | D. WALKER   | Design Check   |          |          |



Client  
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Client DEPARTMENT OF DEFENCE SEG - CFI  
Project EST04824 - P0010 NAW  
HMAS ALBATROSS  
Status PRELIMINARY

Drawing Title DISPLACED THRESHOLD  
AIRCRAFT TRACKING  
RUNWAY 03

Drawing No. 12541981-01-G-SK-3004

Size A1  
Rev B

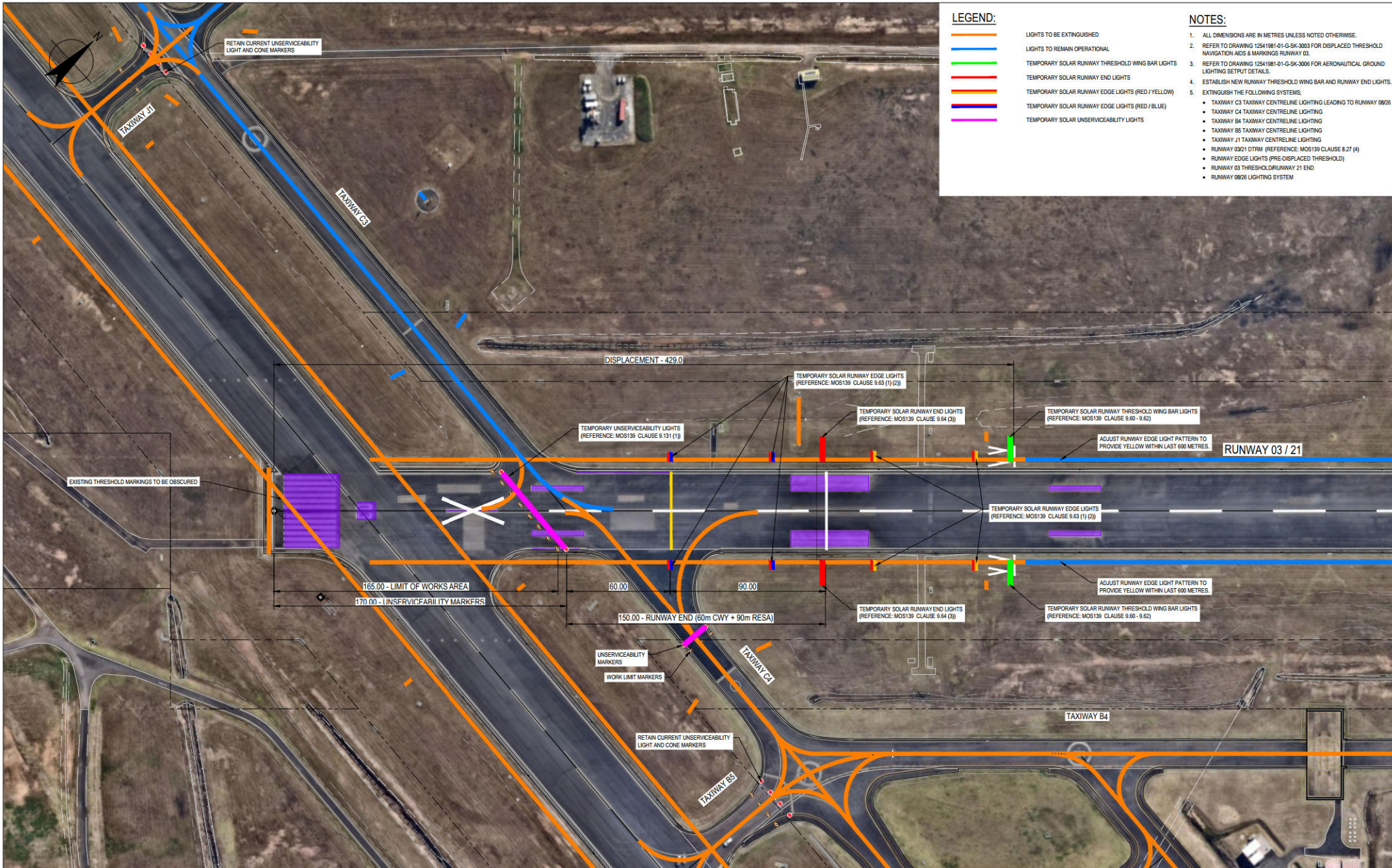
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12541981

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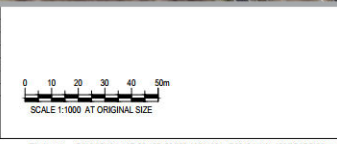
**LEGEND:**

- Orange line: LIGHTS TO BE EXTINGUISHED
- Blue line: LIGHTS TO REMAIN OPERATIONAL
- Green line: TEMPORARY SOLAR RUNWAY THRESHOLD WING BAR LIGHTS
- Red line: TEMPORARY SOLAR RUNWAY END LIGHTS
- Yellow line: TEMPORARY SOLAR RUNWAY EDGE LIGHTS (RED / YELLOW)
- Blue/Red line: TEMPORARY SOLAR RUNWAY EDGE LIGHTS (RED / BLUE)
- Purple line: TEMPORARY SOLAR UNSERVICEABILITY LIGHTS

**NOTES:**

1. ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
2. REFER TO DRAWING 12541981-01-G-SK-3003 FOR DISPLACED THRESHOLD NAVIGATION AIDS & MARKINGS RUNWAY 03.
3. REFER TO DRAWING 12541981-01-G-SK-3006 FOR AERONAUTICAL GROUND LIGHTING SETUP DETAILS.
4. ESTABLISH NEW RUNWAY THRESHOLD WING BAR AND RUNWAY END LIGHTS.
5. EXTINGUISH THE FOLLOWING SYSTEMS:
  - TAXIWAY C3 TAXIWAY CENTRELINE LIGHTING LEADING TO RUNWAY 08/26
  - TAXIWAY C4 TAXIWAY CENTRELINE LIGHTING
  - TAXIWAY B4 TAXIWAY CENTRELINE LIGHTING
  - TAXIWAY B5 TAXIWAY CENTRELINE LIGHTING
  - TAXIWAY J1 TAXIWAY CENTRELINE LIGHTING
  - RUNWAY 03/21 DTRM (REFERENCE: MOS139 CLAUSE 8.27 (4))
  - RUNWAY EDGE LIGHTS (PRE-DISPLACED THRESHOLD)
  - RUNWAY 03 THRESHOLD/RUNWAY 21 END
  - RUNWAY 08/26 LIGHTING SYSTEM

|                   |             |                |          |          |
|-------------------|-------------|----------------|----------|----------|
| B FOR INFORMATION |             | G.F.           | A.H.     | 31.10.24 |
| A FOR INFORMATION |             | G.F.           | D.E.     | 21.08.24 |
| Rev               | Description | Checked        | Approved | Date     |
| Author            | J. MENDOZA  | Drafting Check |          |          |
| Designer          | J. MENDOZA  | Design Check   |          |          |



Client: **Australian Government**  
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Project No:  
**12541981**

Client: **DEPARTMENT OF DEFENCE SEG - CFI**

Project: **EST04824 - P0010 NAW HMAS ALBATROSS**

Status: **PRELIMINARY**

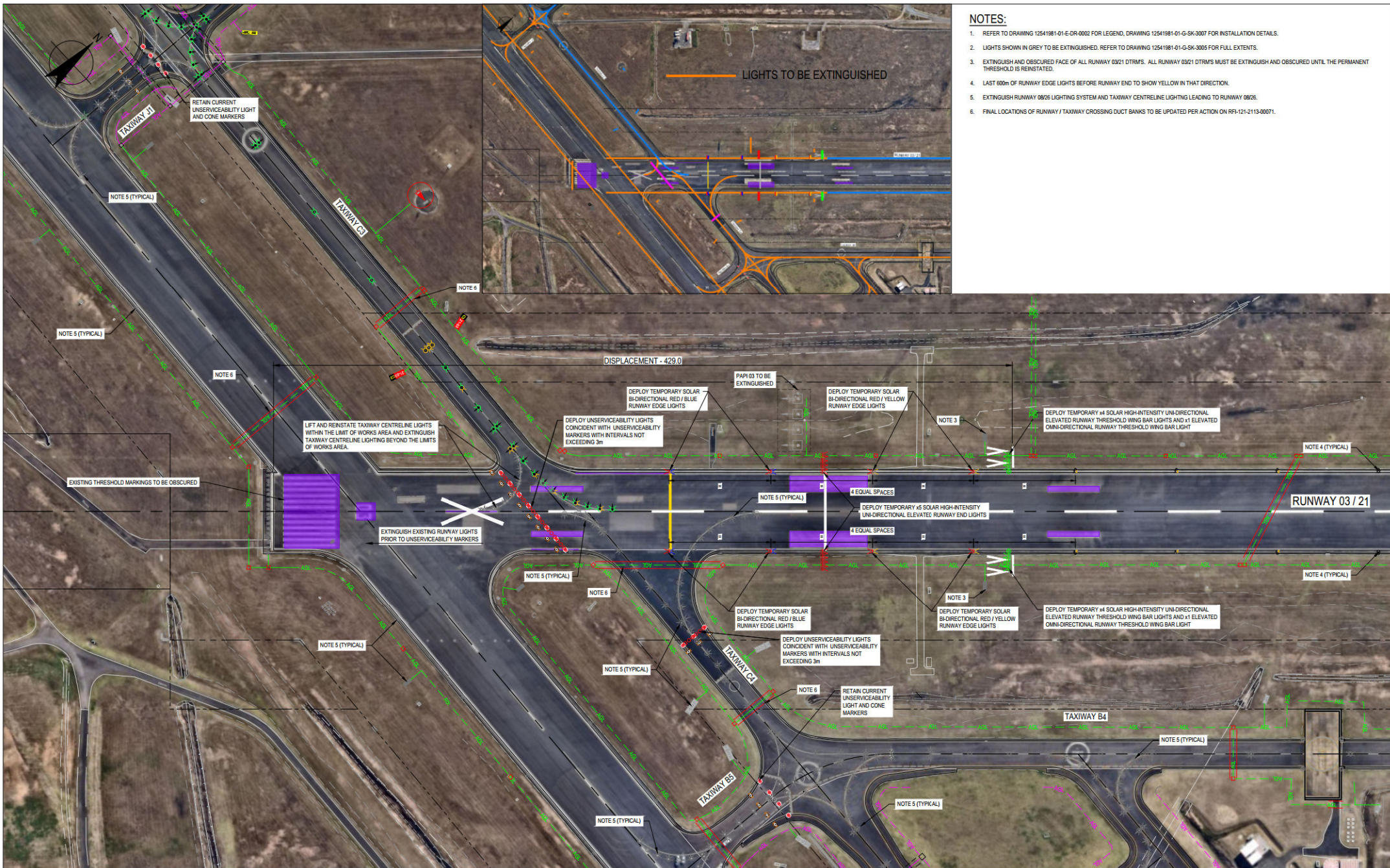
Drawing Title: **DISPLACED THRESHOLD TEMPORARY AGL PLAN RUNWAY 03**

Drawing No: **12541981-01-G-SK-3005**

Size: **A1**

Rev: **B**

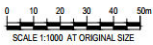




**NOTES:**

1. REFER TO DRAWING 12541981-01-E-OR-0002 FOR LEGEND, DRAWING 12541981-01-G-SK-3007 FOR INSTALLATION DETAILS.
2. LIGHTS SHOWN IN GREY TO BE EXTINGUISHED. REFER TO DRAWING 12541981-01-G-SK-3005 FOR FULL EXTENTS.
3. EXTINGUISH AND OBLISCURED FACE OF ALL RUNWAY 03/21 DTRMS. ALL RUNWAY 03/21 DTRMS MUST BE EXTINGUISH AND OBLISCURED UNTIL THE PERMANENT THRESHOLD IS REINSTATED.
4. LAST 600m OF RUNWAY EDGE LIGHTS BEFORE RUNWAY END TO SHOW YELLOW IN THAT DIRECTION.
5. EXTINGUISH RUNWAY 08/26 LIGHTING SYSTEM AND TAXIWAY CENTRELINE LIGHTING LEADING TO RUNWAY 08/26.
6. FINAL LOCATIONS OF RUNWAY / TAXIWAY CROSSING DUCT BANKS TO BE UPDATED PER ACTION ON RFI-121-2113-00071.

|                   |             |                |          |          |
|-------------------|-------------|----------------|----------|----------|
| B FOR INFORMATION |             | G.F.           | A.H.     | 31.10.24 |
| A FOR INFORMATION |             | G.F.           | D.E.     | 21.08.24 |
| Rev               | Description | Checked        | Approved | Date     |
| Author            | J. MENDOZA  | Drafting Check |          |          |
| Designer          | J. MENDOZA  | Design Check   |          |          |



|             |                                     |
|-------------|-------------------------------------|
| Client      | DEPARTMENT OF DEFENCE SEG - CFI     |
| Project     | EST04824 - P0010 NAW HMAS ALBATROSS |
| Status      | PRELIMINARY                         |
| Project No. | 12541981                            |

**DISPLACED THRESHOLD AERONAUTICAL GROUND LIGHTING SETOUT DETAILS RUNWAY 03**

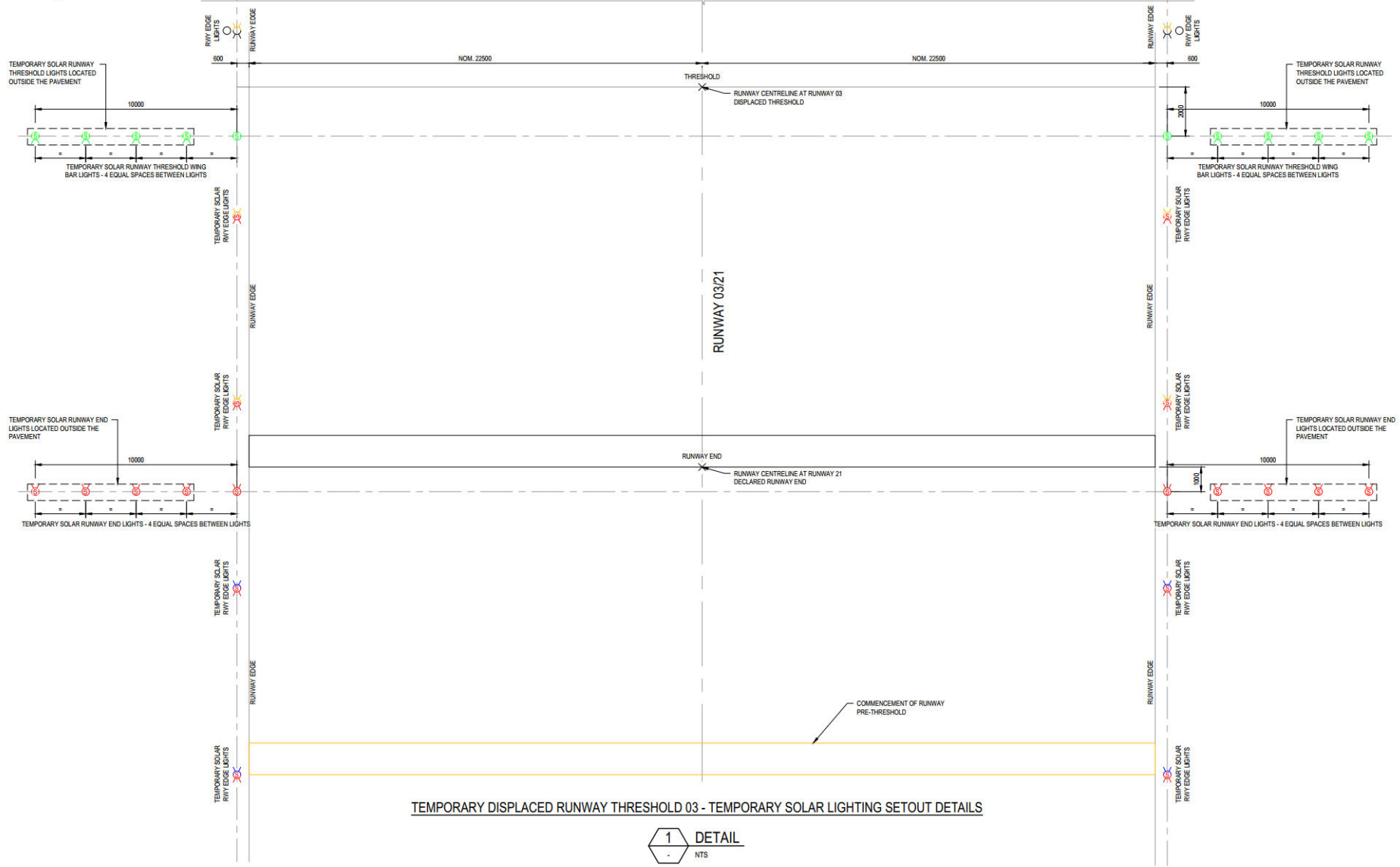
12541981-01-G-SK-3006

Size A1 Rev B



**TEMPORARY THRESHOLD INSTALLATION NOTES:**

- DIMENSIONS IN MILLIMETRES UNLESS SPECIFIED OTHERWISE.
- REFER TO DRAWING 12541981-01-G-SK-3003 FOR DISPLACED THRESHOLD NAVIGATION AIDS & MARKINGS RUNWAY 03.
- CONTRACTOR TO DETERMINE MOUNTING ARRANGEMENT TO SUIT THE TEMPORARY SOLAR LIGHTS. THE PROPOSED MOUNTING METHOD MUST BE SUBMITTED TO THE CONTRACTOR ADMINISTRATOR FOR APPROVAL.
- DESIGN DRAWING REFERENCE: MOS139 CLAUSE 9.60 - 9.62  
MOS139 CLAUSE 9.63 (1)(a)  
MOS139 CLAUSE 9.64 (b)



**TEMPORARY DISPLACED RUNWAY THRESHOLD 03 - TEMPORARY SOLAR LIGHTING SETOUT DETAILS**

1  
-  
NTS  
DETAIL

|                     |                |          |          |
|---------------------|----------------|----------|----------|
|                     |                |          |          |
| B FOR INFORMATION   | G.F.           | A.H.     | 31.10.24 |
| A FOR INFORMATION   | G.F.           | D.E.     | 21.08.24 |
| Rev Description     | Checked        | Approved | Date     |
| Author J. MENDOZA   | Drafting Check |          |          |
| Designer J. MENDOZA | Design Check   |          |          |

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Client

**Australian Government**  
Department of Defence  
Security and Estate Group

PMCA

**Beca**

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|---------|--|
| Client  | DEPARTMENT OF DEFENCE SEG - CFI        |
| Project | EST04824 - P0010 NAW<br>HMAS ALBATROSS |
| Status  | PRELIMINARY                            |

|               |  |
|---------------|--|
| Drawing Title | DISPLACED THRESHOLD<br>AERONAUTICAL GROUND LIGHTING<br>INSTALLATION DETAILS<br>RUNWAY 03 |
| Drawing No.   | 12541981-01-G-SK-3007  |

Size  
**A1**

Rev  
**B**